

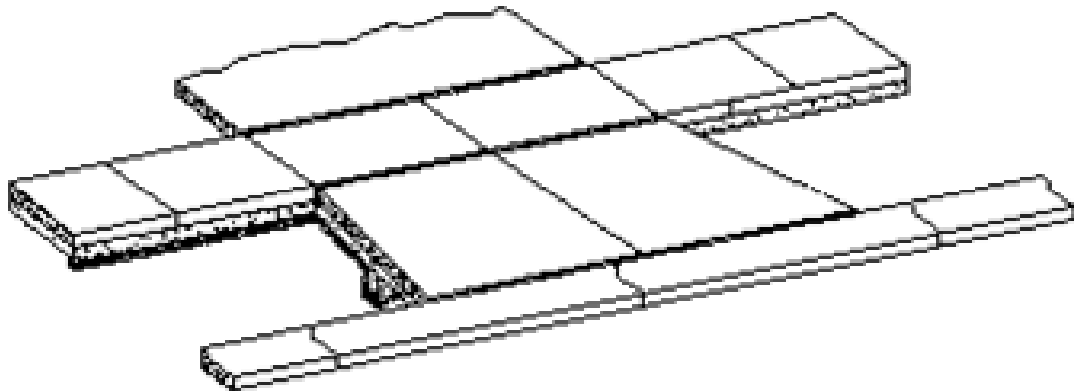


**OAKCREEK**  
— WISCONSIN —

March 2020

**RESIDENTIAL DRIVEWAY APPROACH SPECIFICATIONS**

**CITY OF OAK CREEK  
ENGINEERING DEPARTMENT**



## DRIVEWAY APPROACH SPECIFICATIONS SUMMARY

### GENERAL

- 4" base of compacted 1 ¼" Base Aggregate from street pavement to property line
- 5" of concrete paving
- 3" of asphalt paving
- No wire mesh or rod reinforcing
- Fiber mesh admixture to concrete is allowed

### URBAN STREETS (SEE DETAILS RO-5 AND RO-9)

- Concrete paving only
- Approach slope of 4.0% (See RO-5)
- Sidewalk cross slope of 1.5%
- 3' straight flare on each side of approach from curb to sidewalk section
- Full depth felt joint at curb and both edges of sidewalk

### RURAL "B" STREETS (SEE DETAIL RO-12)

- Concrete or asphalt paving is allowed
- Slope on approach varies from minimum of 2% to maximum of 10%
- 3' straight flare on each side of approach from curb to 8' back of curb
- Full depth felt joint required at curb and 1-foot from right-of-way for concrete approach

### RURAL "A" STREETS (SEE DETAILS RO-10, RO-11 AND RO-13)

- Concrete or asphalt paving is allowed
- 4" base of compacted 1 ¼" Base Aggregate from edge of street pavement to property line
- 3' straight flare on each side of approach from curb to centerline of culvert
- Slope for first 3' of approach to match cross-slope of the street pavement (See RO-13)

### RURAL "A" STREETS WITH ONLY THE BINDER COURSE OF ASPHALT IN PLACE

- Same as above except first 3' of approach for a concrete driveway
- Approach will end 3' short of edge of street pavement
- End of approach will be set level with the existing edge of street pavement

### GENERAL PROVISION HIGHLIGHTS

- (4) Permits – Required for all new installations, replacements, and repair of driveways and sidewalks
- (15) Minimum Clearances
  - To side property line 5 feet
  - Fire hydrant, mailbox, light pole 5 feet
  - Street Intersection 40 feet
- (19) Culverts – Installed and extended by City Street Department only (768-6553)
- (20) Culvert Headwalls and Curb on Driveway Approach – Allowed on rural streets
- (22) Depressed Curb
  - Required for apartments and all other commercial approaches
  - Allowed for residential driveways on curbed street pavements
  - Creation of a depressed curb through the sawing off of the existing curb head may be allowed. The curb cut will be inspected all at once with the rest of the approach, no separate inspection solely for the curb cut. As indicated on the permit application form, only equipment and personnel designed and qualified to perform this specific work are acceptable to the City.
- (27) 24-hour turnaround on inspection requests (24 hours from time approach is ready to be inspected)
- (41) Driveway approaches disturbed by City projects will be replaced with standard concrete or asphalt, where applicable, with no consideration for matching pre-existing color, texture or pattern.

## **GENERAL PROVISIONS**

- 1 - Scope of Work
- 2 - Definition of Public Right-of-Way
- 3 - Financial Responsibility
- 4 - Permit to Construct in Public Right-of-Way
- 5 - Driveway Approach along an Interim Street Section
- 6 - Variances
- 7 - Deficiencies
- 8 - Workmanship
- 9 - Stop Permit Order
- 10 - Stop Work Order
- 11 - Revocation of Permit
- 12 - Diggers Hotline Locate
- 13 - Cleaning of Pavement and Right-of-Way
- 14 - Clearing and Grubbing
- 15 - Minimum Clearances
- 16 - Adjustment of Interfering Appurtenances
- 17 - Protection of Structures and Utilities
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- 20 - Headwalls at Culvert Ends
- 21 - Approach Installed Prior to Completion of Asphalt Street Paving
- 22 - Depressed Curb
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- 24 - Preparation of Subgrade
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- 26 - Setting of Forms
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- 29 - Asphalt Specifications
- 30 - Concrete Specifications
- 31 - Joint Treatment
- 32 - Sawcutting Pavement
- 33 - Placing, Finishing and Curing Concrete
- 34 - Concrete Protection
- 35 - Backfilling and Restoration
- 36 - Barricades, Signs and Warning Devices
- 37 - Closing of Street
- 38 - Material Storage on Street
- 39 - Water Use Permit
- 40 - Mudjacking Repair
- 41 - Colors, Textures, Patterns

## **DEFINITION OF TERMS**

Appendix "A"

## **DETAIL DRAWINGS**

Appendix "B"

## GENERAL PROVISIONS

### 1. SCOPE OF WORK

These specifications control the repair, removal, installation, and replacement of residential driveway approaches and sidewalks constructed within the right-of-way of public streets in the City of Oak Creek.

### 2. DEFINITION OF PUBLIC RIGHT-OF-WAY

The area designated for public streets, sidewalks and utilities measured between opposing property lines. This is commonly 60' or 66', but may be greater. The distance from the center of pavement to the right-of-way will generally be 30' or 33'. Facilities typically included within the public right-of-way are street pavement, curb & gutter or road shoulder, ditches, driveway culverts, driveway approaches, terrace areas, and sidewalks.

Public utilities and facilities located in the right-of-way are sanitary sewer, storm sewer, water main, street lighting, and street trees. Private utilities include natural gas, electric, telephone, cable TV, and fiber optic lines.

### 3. FINANCIAL RESPONSIBILITY

All private residential paving improvements shall be constructed and financed entirely by the property owner. All necessary cleanup and restoration shall be performed and paid for by the property owner or hired contractor. All barricades, signs, lights, and other necessary warning devices shall be installed, maintained, and paid for by the property owner or hired contractor.

### 4. PERMIT TO CONSTRUCT IN PUBLIC RIGHT-OF-WAY

A permit is required to repair, remove, install, or replace a driveway approach or sidewalk. The permit shall be obtained from the City of Oak Creek Engineering Department before the start of any construction activities. Starting work before the issuance of a permit shall result in the issuance of a stop work order.

### 5. DRIVEWAY APPROACH ALONG AN INTERIM STREET SECTION

Any driveway approach to be installed on a street with an interim cross-section is subject to the special stipulation that the property owner shall be held financially responsible for the repair or replacement of the approach should it become necessary due to any future City-generated construction project within the street right-of-way.

### 6. VARIANCES

Variations shall be requested in writing, addressed and submitted to the City Engineer. At their discretion, the City Engineer may approve the requested variance.

### 7. DEFICIENCIES

Any work performed in violation of these specifications, City ordinances, or orders of a City of Oak Creek officer, may be ordered removed. Any work not in compliance with the City standards may be ordered to be removed or replaced.

### 8. WORKMANSHIP

Defects due to poor workmanship or materials shall be cause to order work to be removed or replaced. Spalling, poor surface finish, damage caused by equipment or neglect, and cracking of materials are examples of cause.

**9. STOP PERMIT ORDER**

Flagrant violation of rules, specifications, or requirements shall invoke possible application of a stop permit order on the contractor. The application of this order will allow the contractor to complete work on permits previously issued, but shall bar the contractor from acquiring new permits or starting new work from the date and time of the stop permit order. The order shall be issued to, and served upon, the contractor by U.S. Mail, email, posted on site or delivered in person. The contractor must formally resolve the violation with the City Engineer in order to request the lifting of the stop permit order. At their discretion, the City Engineer will decide the appropriate course of action.

**10. STOP WORK ORDER**

A property which is posted with a stop work order will have no further work performed, on or adjacent to it, until such time as the stop work order is lifted by a City officer.

**11. REVOCAION OF PERMIT**

The City Engineer may revoke a permit for cause.

**12. DIGGERS HOTLINE LOCATE**

The contractor shall be responsible for having utilities located before beginning any construction-related activities.

**13. CLEANING OF PAVEMENT AND RIGHT-OF-WAY**

The contractor shall be responsible for removing all equipment and debris and for sweeping all affected pavement within the public right-of-way. The contractor shall perform this cleanup prior to leaving the site for the day. If contractor does not fulfill this requirement and City forces are ordered to perform this work, the contractor shall be billed for the associated costs.

**14. CLEARING AND GRUBBING**

The contractor shall remove and dispose of trees, stumps, roots, vegetation, topsoil, rubbish, or other refuse found within the public right-of-way portion of the work zone. Rocks and boulders in this area shall be removed as well. Tree stumps 3" and greater in diameter shall be removed to a depth of not less than 1' foot below the pavement subgrade.

**15. MINIMUM CLEARANCES**

Clearances from the driveway approach, as measured at the front property line, to other structures, shall be as follows:

To Side Property Line	5 feet <sup>(1)</sup>
Fire Hydrant	5 feet
Mail Box	5 feet
Street Light Pole	5 feet
Street Intersection	40 feet <sup>(2)</sup>

<sup>(1)</sup> Property owner shall be responsible to field locate the side property line.

<sup>(2)</sup> The 40' dimension as measured along the property line from the edge of proposed driveway approach to the property line along the frontage of the intersecting street. Property owner shall be responsible for locating the relevant property corner.

**16. ADJUSTMENT OF INTERFERING APPURTENANCES**

All manhole and catch basin frames, and all covers for openings of any kind encroaching into the driveway approach and sidewalk areas shall be adjusted to the appropriate line and grade. The City will adjust, at no charge to the contractor, any item that is City property. The City will make the adjustment within 72 hours of receiving the request from the property owner or contractor.

When adjustments involve a private utility, the Contractor shall contact the private utility involved. Adjustments shall be made according to the requirements of the private utility.

Fire hydrants and street lights, which are approved for relocation by the City Engineer to accommodate the driveway approach installation, shall be relocated by the City with all related costs being the responsibility of the property owner.

**17. PROTECTION OF STRUCTURES AND UTILITIES**

The contractor shall report existing damage or other deficiencies of public and private utilities, culverts, trees, light poles, pavement, valve boxes, stop boxes, covers, curb, gutter, road shoulder, ditches, driveway approaches, terrace areas, and sidewalks, etc. The contractor shall notify the appropriate authority of the damage or deficiency.

Public utilities and facilities located in the right-of-way include sanitary sewer, storm sewer, water main, street lighting, and street trees.

Private utilities include natural gas, electric, phone, cable TV, and fiber optic lines.

The contractor shall protect all existing work from damage including underground structures, public and private utilities, culverts, trees, light poles, pavement, valve boxes, stop boxes, covers, curb, gutter, road shoulder, ditches, driveway approaches, terrace areas, and sidewalks, etc. All damaged items shall be repaired prior to the pouring of concrete or placing of asphalt. The contractor shall be charged for the repair costs for all items they have damaged and for which they are responsible.

**18. CLEANING STRUCTURES AND COVERS**

The contractor shall be responsible to ensure that all manholes, catch basins, valve boxes, pull-boxes (and their associated covers) in the work area are clean and operable prior to and upon completion of the work.

**19. CULVERTS**

The City of Oak Creek Street Department issues the permit for installation of private roadside driveway culverts. Culverts are installed by the City Street Department only. The material and installation costs of the culvert are paid for by the property owner or their home builder.

Culverts shall be a minimum of 20 feet in length and a maximum of 40 feet. Fill material over culverts shall be crushed stone to subgrade. A minimum of 4" of 1 ¼" Base Aggregate material shall be placed and compacted over the stone backfill material.

**20. HEADWALLS AT CULVERT ENDS**

Headwalls may be constructed to protect the driveway approach from erosion and undermining at the culvert ends. Headwalls may be constructed of formed concrete or masonry laid and secured in place. Curb sections may be no closer than 6' from the edge of the street pavement. The height of the curb shall be no more than 6" above the approach paving.

**21. APPROACH INSTALLED PRIOR TO COMPLETION OF ASPHALT STREET PAVING**

Concrete driveway approaches that are to be installed on streets with a Rural-A cross-section (no curb & gutter) where the final course of asphalt has not yet been placed shall not be paved all the way to the street. The approach shall stop 3' short of the street pavement edge and to an elevation level with the current street pavement edge. (See Details RO-11 and RO-13)

**22. DEPRESSED CURB**

A depressed curb section for a driveway approach may be installed only with the prior approval of the City Engineer. With the City Engineer's approval, the existing mountable curb & gutter section may be removed to the extent approved by the on-site Inspector. In no case may a section of curb & gutter be created with a length less than 4'. Joints shall be sawed at the removal extents to the full depth of the curb & gutter prior to its removal. New curb section shall be tied to adjacent existing curb & gutter with steel tie bars. (See Detail Number RO-7)

Creation of a depressed curb through the sawing off of the existing curb head may also be allowed. The curb cut will be inspected all at once with the rest of the approach, no separate inspection solely for the curb cut. As indicated on the permit application form, only equipment and personnel designed and qualified to perform this specific work are acceptable to the City.

**23. ALIGNMENT OF APPROACHES & SIDEWALKS WITHIN ROW**

Where the edge of the public street is on a straight line, the centerline of the driveway approach shall be at a 90° angle to that line. Where the edge of the public street is within a horizontal curve, the centerline of the driveway approach shall be radial to that curve.

**24. PREPARATION OF SUBGRADE**

All areas in the subgrades that are not firm and of proper bearing capacity shall be removed and backfilled with compacted gravel or stone. Fill materials shall be placed in layers of no more than 8" and compacted through the use of mechanical tamping equipment so that a firm, stable subgrade is achieved.

**25. PREPARATION OF BASE**

The base course for all paving shall consist of 4" of 1 ¼" Base Aggregate placed upon the subgrade. The base course shall be compacted by mechanical tamping. The contractor shall ensure that the base course is at the proper elevation and grade.

**26. SETTING OF FORMS**

The contractor shall set concrete forms to the proper line and grade. The forms shall be properly staked and braced.

**27. REQUEST FOR INSPECTION**

Inspections shall be requested after all preparatory work has been completed (preparation of the base course, setting of forms, etc.). The request for inspection shall be made to the Engineering Department (766-7000). No concrete or asphalt shall be placed prior to completion of the City inspection and approval. Concrete and asphalt should not be ordered prior to the Inspector's approval of the stone grade and forms.

Inspections will be performed within 24 hours from the time that all preparatory work has been

completed. The 24-hour response time refers to normal workdays of City personnel (i.e. not including weekends or City holidays). Failed inspections will require a new request for inspection upon correction of the deficiencies.

**28. AIR TEMPERATURE**

No asphalt shall be placed when the air temperature is below 36° Fahrenheit. No concrete shall be placed when the temperature is below 40° Fahrenheit. In cold weather, all necessary insulating precautions shall be employed, as specified by State of Wisconsin Specifications, to prevent freezing of the concrete during the curing process.

Predicted or Actual Air Temperature	Minimum Equivalent Level of Protection
22 to <28 F	Single layer of polyethylene
17 to <22 F	Double layer of polyethylene
<17 F	6" of loose, dry straw or hay between 2 layers of polyethylene

**29. ASPHALT SPECIFICATIONS**

A minimum of 3" of asphalt with a minimum gradation of 9.5mm LT or better, not to exceed 12.5 mm, as specified by State of Wisconsin Specifications, shall be placed upon the prepared base course.

**30. CONCRETE SPECIFICATIONS**

All concrete used shall adhere to the following requirements:

AIR ENTRAINED	air content of 5% to 7% by volume
PORTLAND CEMENT	6 bags per cubic yard
COARSE AGGREGATE	maximum size of 1-1/2"
SLUMP	4" or less

The exact proportions of water, fine aggregate, and coarse aggregate, shall be designed to produce concrete of the desired plasticity, workability, strength, and durability.

**31. JOINT TREATMENT**

A longitudinal contraction joint shall be tooled in at the centerline of the approach. Installation of additional joints may be directed by the Inspector. For concrete sidewalk, transverse contraction joints shall be provided at 5' to 6' intervals. For concrete driveway approaches, a transverse joint shall be provided when the distance between the walk and the back of curb exceeds 12'. Saw cut joints will only be allowed when the joint can be cut to the required depth across the entire length of the pavement section without causing damage to abutting pavement and/or curb section.

All full-depth joints shall be installed using 5" deep, 1/2" thick bituminous fiber expansion joint material. Full-depth joints shall be installed at the back of curb, between the driveway approach and sidewalk section, between the sidewalk section and private driveway, and where a new walk abuts an existing walk, as directed by the Inspector.

**32. SAWCUTTING PAVEMENT**

Prior to removing existing pavement (concrete or asphalt), the limits of the removal shall be clearly indicated and approved by the Inspector. Joints shall be sawed for the full depth of the pavement at the approved locations and shall provide a clean and square edge.



**33. PLACING, FINISHING AND CURING CONCRETE**

The base course shall be moistened before concrete is placed. All edges shall be finished with a ½" radius edging tool. Contraction joints shall be tooled in cut to a depth of 1¾". The final surface finish shall be obtained by brushing lightly with a damp, soft-bristled brush. No neat cement or water shall be applied to the surface during finishing. Immediately after finishing the concrete surface, the approach shall be uniformly covered with a water-impermeable curing compound. The curing compound shall be applied as a fine spray at a coverage rate of 200 square feet per gallon.

**34. CONCRETE PROTECTION**

In addition to protection from inclement weather, concrete paving shall be closed to pedestrian traffic for 12 hours and closed to vehicular traffic for 7 days.

**35. BACKFILLING AND RESTORATION**

As soon as the new pavement strength will allow and after the concrete forms have been removed, the areas adjacent to the new pavement shall be backfilled and shaped to the proper finish elevations. Seed and mulch or sod restoration shall be established. All work-related debris shall be removed from the public right-of-way. All street and sidewalk areas shall be cleaned and free of hazards.

**36. BARRICADES, SIGNS, AND WARNING DEVICES**

The contractor shall erect and maintain all barricades, fences, lights, and signs necessary for public safety and convenience. New concrete pavement shall be closed to traffic during the construction and curing periods by the placement of sufficient barricades and/or warning devices. The City may order the placement of additional safety devices where it sees necessary. The contractor shall be liable for damage caused by failure to install and maintain adequate safety devices.

**37. CLOSING OF STREET**

In no event shall the contractor close the street to vehicular traffic or access of the general public throughout the construction. At least one 12' lane of pavement shall remain open to vehicular traffic throughout all construction activities. When just one through lane of traffic is available, Contractor shall flag two-way traffic through the construction zone.

**38. MATERIAL STORAGE ON STREET**

Materials delivered and stored on any street shall be neatly piled along roadway sides in such a manner as to minimize inconvenience to property owners, vehicular traffic, and the general public. Materials shall not be placed within 20' of any public hydrant. Private drives shall be kept open. Trees and other improvements shall be protected from damage. Barricades with flashing lights will be set to warn of materials stored on the street pavement. The City may order the placement of additional safety devices.

Material storage on street shall be limited to one working day. Obtain a permit for Temporary Work Encroachment Within Public Right-Of-Way from the City of Oak Creek if materials will be stored on the street pavement exceeding one working day.

**39. WATER USE PERMIT**

The contractor shall not open, close, or operate any hydrant valve. Fire hydrants may be used as source of water supply only after obtaining a permit from the Oak Creek Water and Sewer Utility located at 170 W. Drexel Avenue (570-8210). When water is required, the Utility shall be notified. The Utility shall install the necessary reducer, valve, etc., and shall open the hydrant valve.

**40. MUDJACK REPAIR OF EXISTING APPROACH**

Mudjacking is intended to raise, level, or bring to grade selected sidewalk and pavement slabs. Mudjacking shall include mudhole drilling, joint sawing when required, jacking, mudhole plugging, restoration and cleanup.

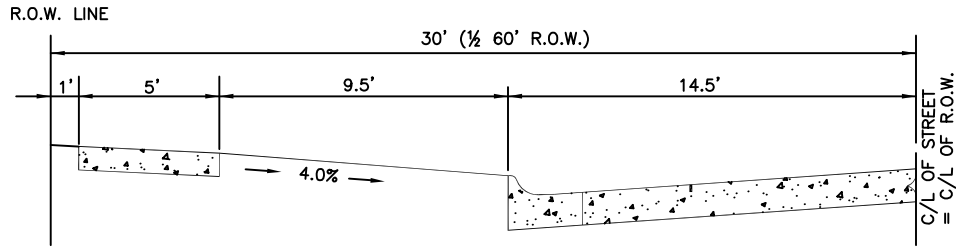
**41. COLORS, TEXTURES, PATTERNS**

Pavements constructed with non-standard colors, textures or patterns that are subject to removal/replacement due to a City project will be reconstructed only to standard concrete or asphalt.

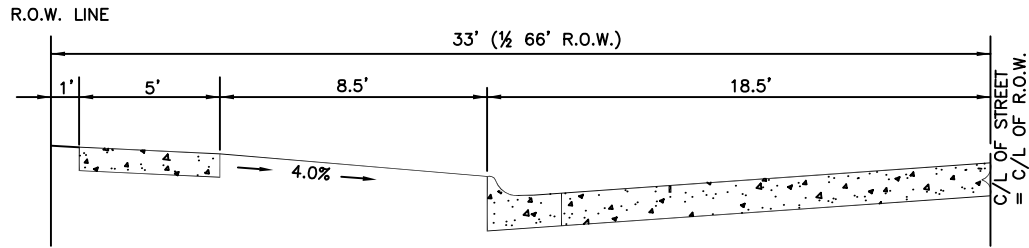
## **APPENDIX "A" - DEFINITION OF TERMS**

Barrier Curb.....	A curb section with a vertical face, height of 6" or more.
Base Course.....	The subbase of compacted crushed stone material upon which the asphalt or concrete is constructed.
City Engineer.....	The City Engineer or their designated representative.
Depressed Curb .....	A curb section whose curb line is 1" above the gutter line (See Detail RO-7).
Driveway Approach .....	That portion of a private driveway that is located within the public right-of-way. Its sole intended purpose is to provide ingress and egress for vehicles to private property.
Flare .....	The tapered section of a driveway extending from the curb to the edge of the driveway.
Interim Street Section.....	A public street in which the ultimate design improvements have not yet been installed.
Line and Grade.....	The horizontal line or dimensions of the work as specified and approved. The grade is the elevation of the work as specified and approved.
Mountable Curb.....	A curb section with a rolled-face profile, generally 4" in height from the gutter.
Neat Cement .....	The raw, powdered Portland Cement as taken directly from the bag.
Residential Driveway.....	A pavement providing vehicle access to a private residence.
Right-of-Way .....	Publicly-owned land for the purpose of vehicular, pedestrian, or utility use.
Rural-A Roadway .....	A public street designed with gravel shoulder and roadside ditches.
Rural-B Roadway .....	A public street designed with mountable curb & gutter, storm sewer and street lighting.
Side Yard Setback.....	The driveway will be located a minimum of 5' from a side property line.
Subgrade.....	The original ground material as prepared to accept the placement of the base course.
Terrace Area .....	The grassed area between the back of the curb and the sidewalk (or the property line if there is no sidewalk installed).
Tooled Joint.....	A contraction joint that is worked into concrete by use of a jointing tool before the concrete has set up. This joint is created during of the concrete finishing process.
Traffic-Bond Material.....	A crushed stone material containing a significant percentage of fines.
Urban Roadway.....	A public street designed with curb & gutter (either mountable or vertical-face "barrier"), storm sewer, sidewalks, street lighting, and street trees.

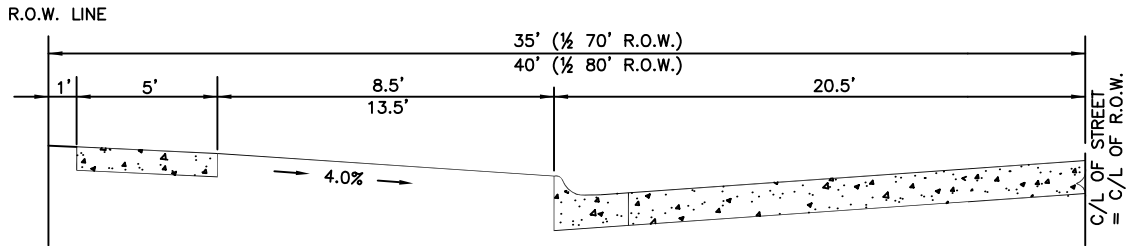
**APPENDIX "B" – DETAIL DRAWINGS**



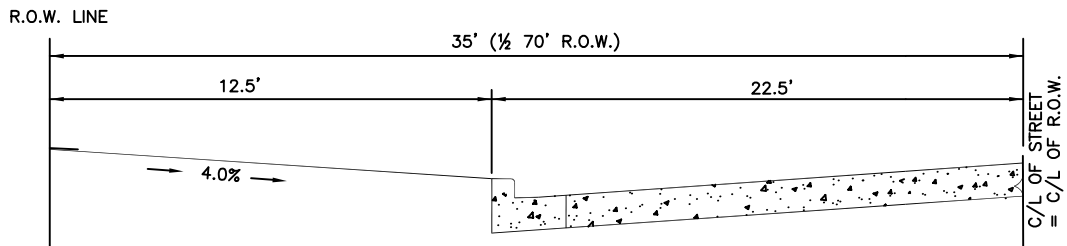
TYPICAL RESIDENTIAL STREET CROSS-SECTION 60' STREET R.O.W.  
29' PAVEMENT WITH CURB AND GUTTER



TYPICAL RESIDENTIAL STREET CROSS-SECTION 66' STREET R.O.W.  
37' PAVEMENT WITH CURB AND GUTTER



TYPICAL RESIDENTIAL STREET CROSS-SECTION 70' & 80' STREET R.O.W.  
41' PAVEMENT WITH CURB AND GUTTER



TYPICAL COMMERCIAL STREET CROSS-SECTION 70' STREET R.O.W.  
45' PAVEMENT WITH CURB AND GUTTER



City of Oak Creek  
Department of Public Works

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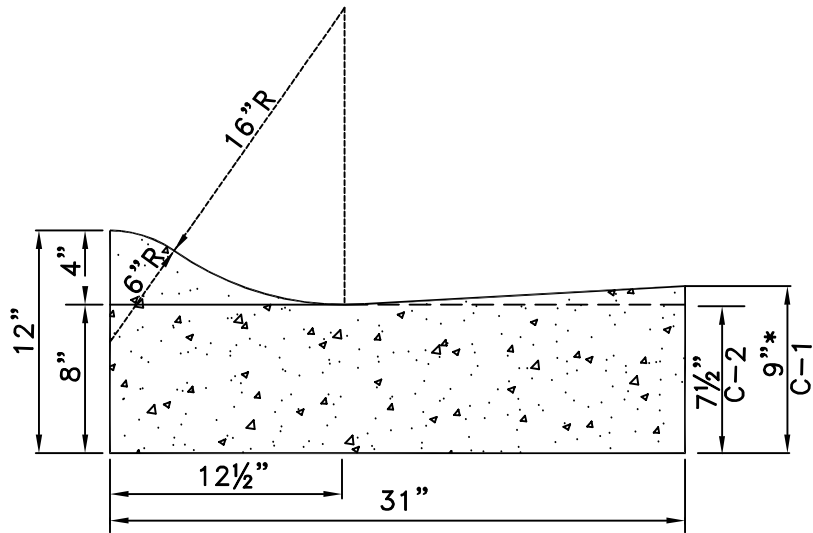
Appv. By: M. SIMMONS

Date: 3/6/20 (Revised)

TYP. URBAN CROSS-SECTION DIMENSIONS

RO-5

## MOUNTABLE – TYPE "C" CONCRETE CURB & GUTTER

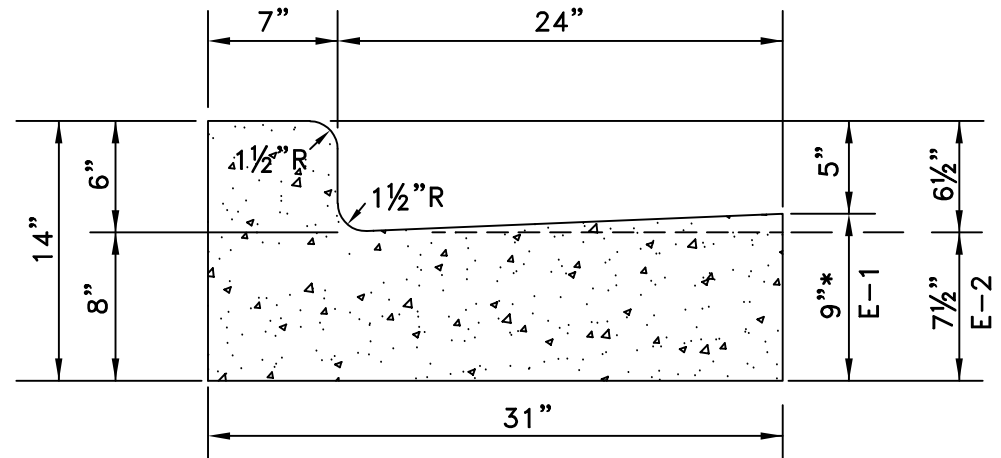


### CURB & GUTTER TYPE

TYPE C-1	LOW-SIDE CURB & GUTTER
TYPE C-2	HIGH-SIDE CURB & GUTTER

\* WHEN USED WITH CONCRETE PAVEMENT,  
CURB & GUTTER THICKNESS SHALL BE  
EQUAL TO THE PAVEMENT THICKNESS BUT  
NOT LESS THAN 7".

## VERTICAL FACE – TYPE "E" CONCRETE CURB & GUTTER



### CURB & GUTTER TYPE

TYPE E-1	LOW-SIDE CURB & GUTTER
TYPE E-2	HIGH-SIDE CURB & GUTTER



*City of Oak Creek*  
*Department of Public Works*

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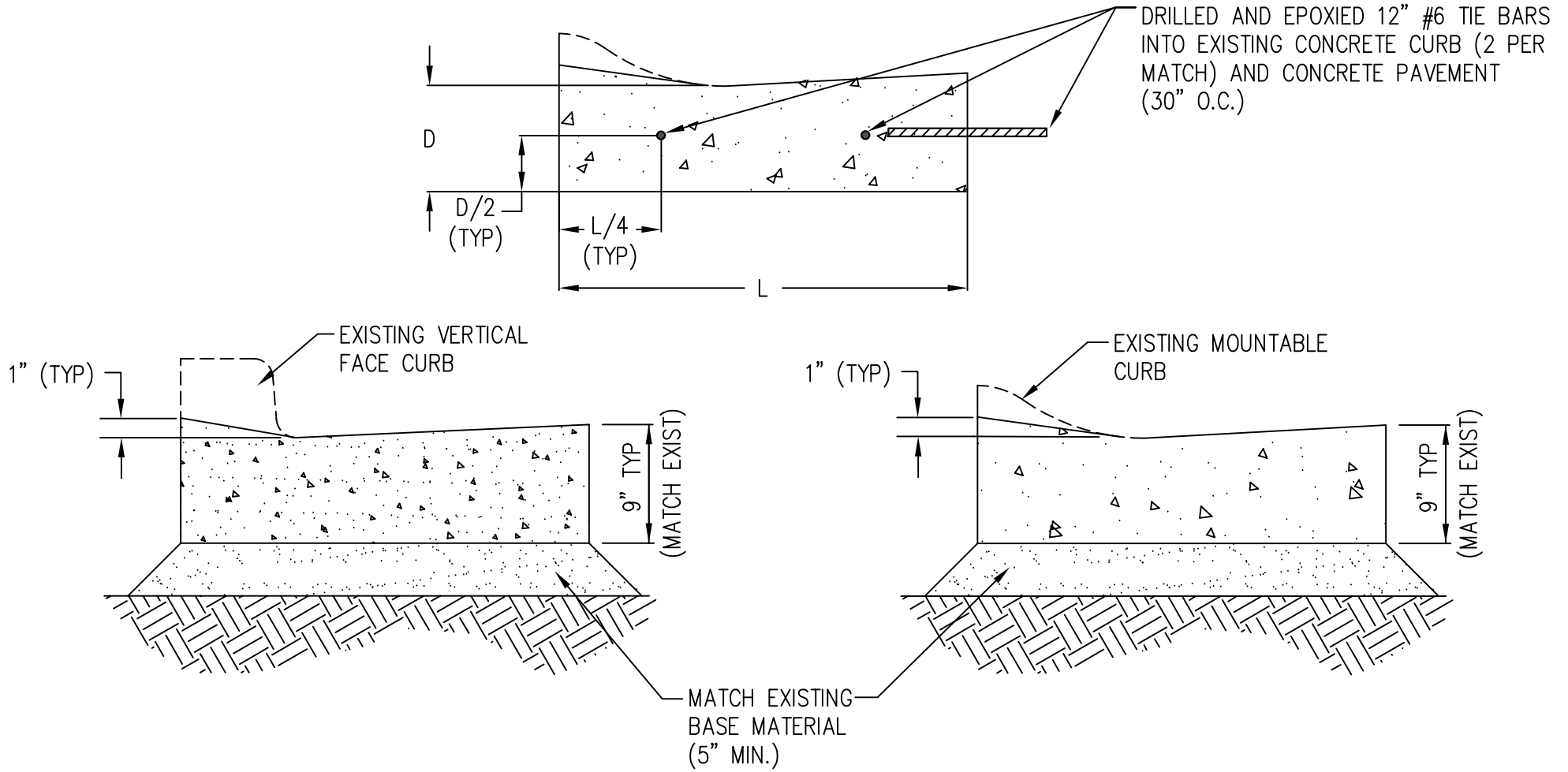
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Appv. By: M. SIMMONS

Date: 3/6/20 (REVISED)

CONCRETE CURB & GUTTER

RO-6



EXCEPT AS NOTED HERE, THIS CURB SHALL BE IN SUBSTANTIAL CONFORMANCE WITH DETAIL RO-6.



City of Oak Creek  
Department of Public Works

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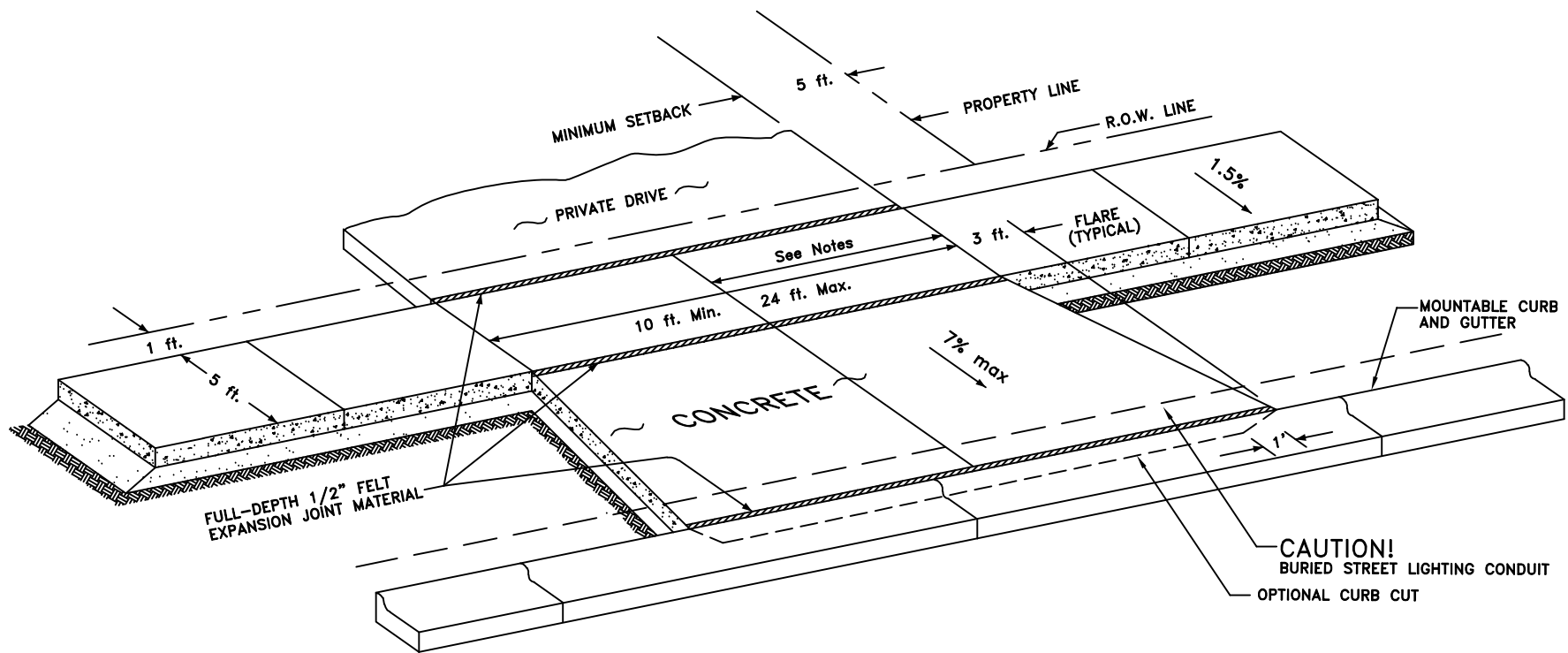
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Appv. By: M. SIMMONS

Date: 3/6/20 (Revised)

DEPRESSED CURB DETAIL

RO-7



**NOTES**

All concrete to be 5 inches thick placed upon 4 inches of compacted 1 1/4" base aggregate.

Concrete shall have no colors, texture stamping or steel reinforcement. Fiber reinforcement is allowed.

Sidewalk joints shall be uniformly spaced with joints no farther than 6'-6", nor less than 4'-8" apart.

Vertical or rolled face curb shall be removed and replaced with depressed curb. At the property owner's option, the existing curb head may be sawed off (City approval must be obtained first) to provide a depressed curb entrance (see Depressed Curb Detail, RO-7). Mountable curb may be removed and replaced with depressed curb at the owner's expense.

Replaced curb shall be installed in accordance with City of Oak Creek specifications including, but not limited to: removal limits, construction of curb return, tie bar installation, and joint spacing. Depressed curb shall have 1' tapers to match existing curb head.

Sidewalk cross slope is 1.5%, +/- 0.5%. Cross slope shall not exceed 2.0%



*City of Oak Creek*  
*Department of Public Works*

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Scale: None

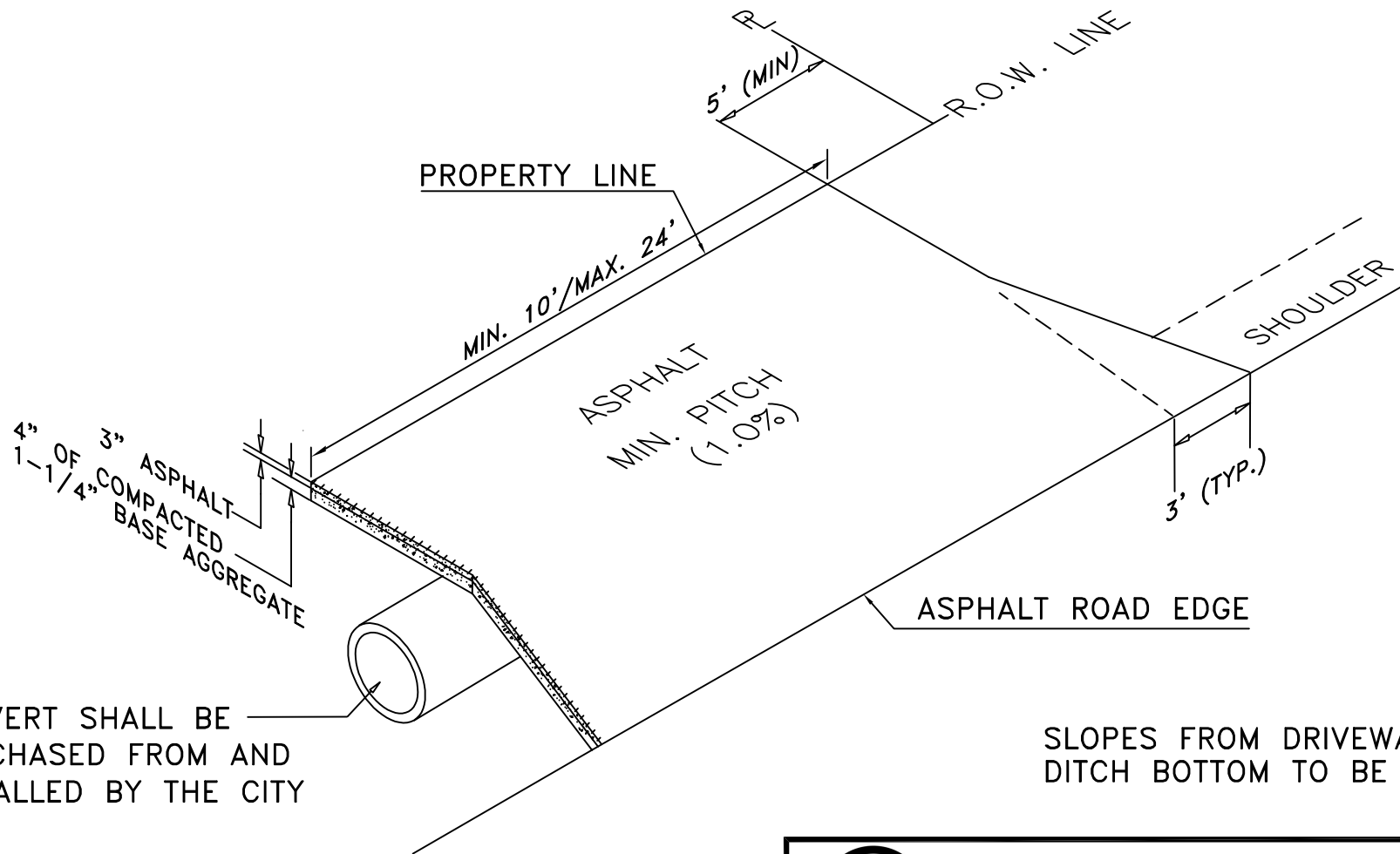
Appv. By: M. SIMMONS

Date: 3/6/20 (Revised)

URBAN DRIVEWAY APPROACH

RO-9





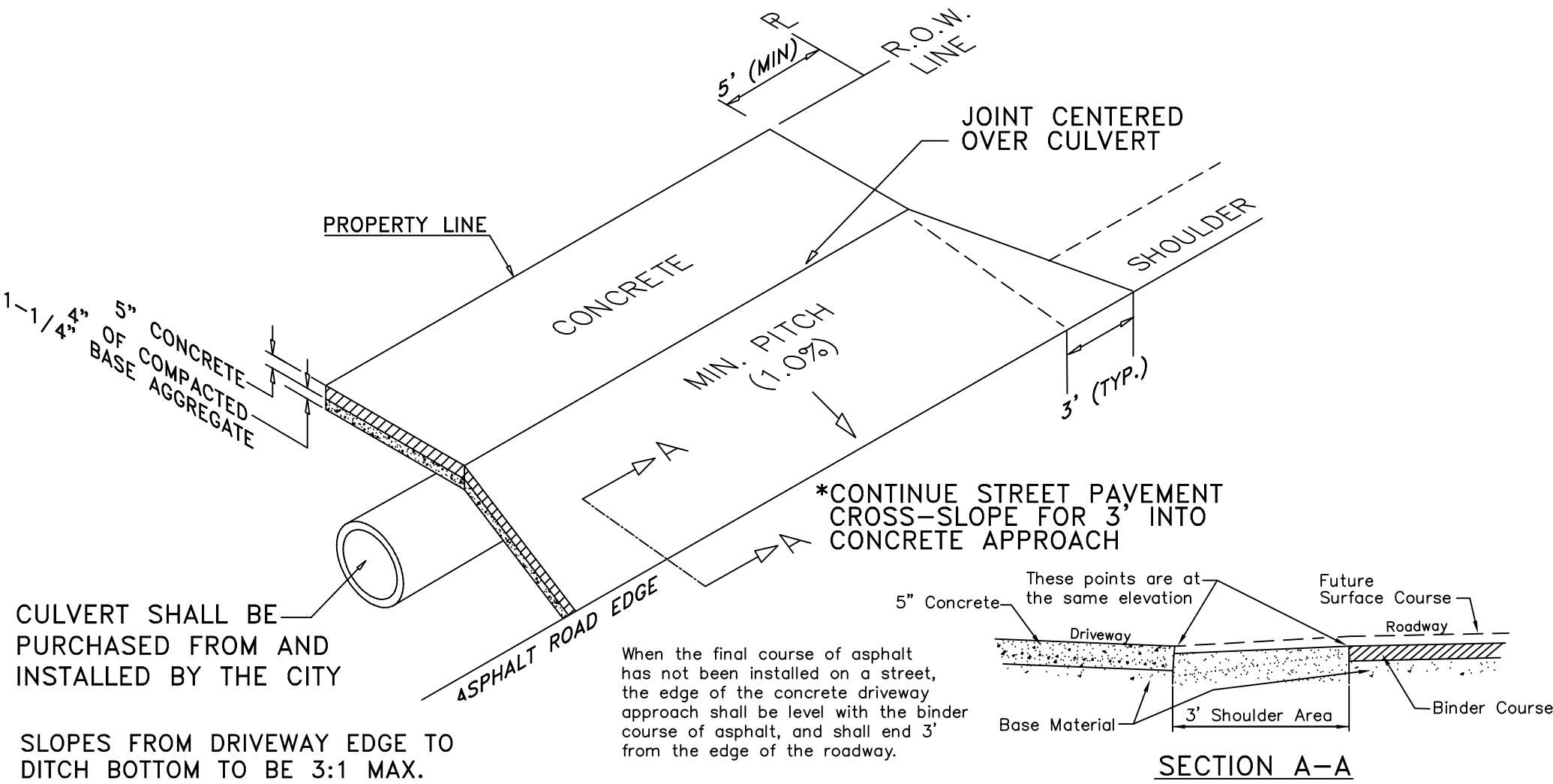
CULVERT SHALL BE PURCHASED FROM AND INSTALLED BY THE CITY

SLOPES FROM DRIVEWAY EDGE TO DITCH BOTTOM TO BE 3:1 MAX.



*City of Oak Creek*  
*Department of Public Works*

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Appv. By: M. SIMMONS	Date: 3/6/20 (Revised)
RURAL-A DRIVEWAY APPROACH (ASPHALT) RO-10	



City of Oak Creek  
 Department of Public Works

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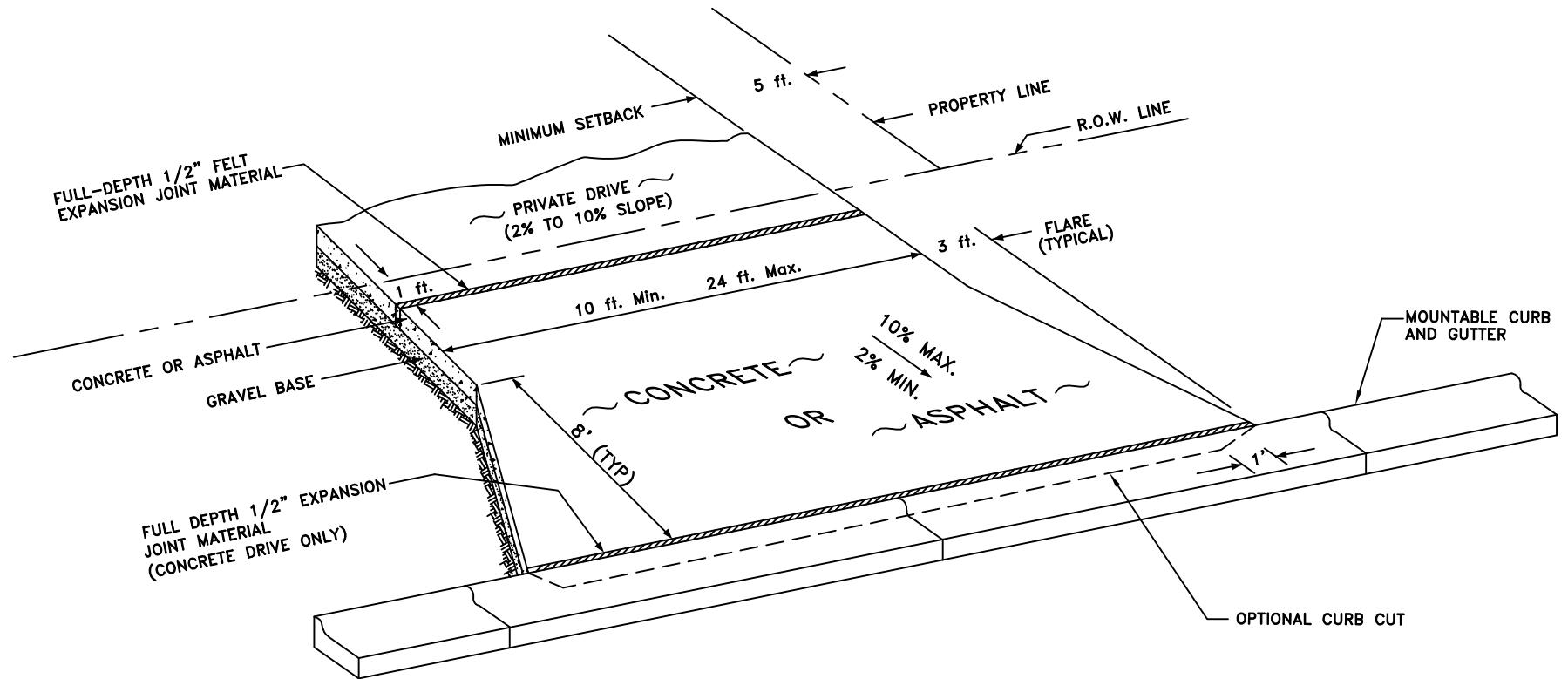
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Appv. By: M. SIMMONS

Date: 3/6/20

RURAL-A DRIVEWAY APPROACH (CONCRETE)

RO-11



**NOTES**

All concrete (5") and asphalt (3") shall be placed on 4 inches of compacted 1 1/4" base aggregate.

Vertical or rolled face curb shall be removed and replaced with depressed curb (see Depressed Curb Detail, RO-7). At the property owner's option, the existing curb head may be sawed off (City approval must be obtained first) to provide a depressed curb entrance. Mountable curb may be removed and replaced with depressed curb at the owner's expense.

Replaced curb shall be installed in accordance with City of Oak Creek specifications including, but not limited to: removal limits, construction of curb return, tie bar installation, and joint spacing. Depressed curb shall have 1' tapers to match existing curb head.



*City of Oak Creek*  
*Department of Public Works*

Drawn By: A. WHITE

Scale: None

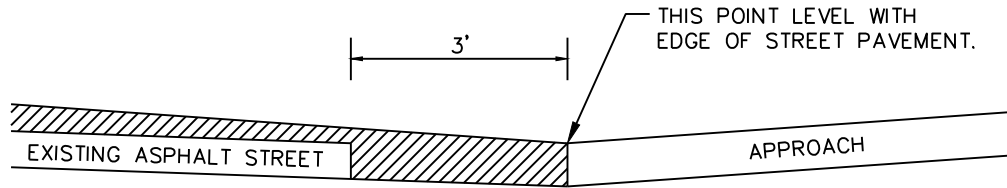
Appv. By: M. SIMMONS

Date: 3/6/20 (Revised)

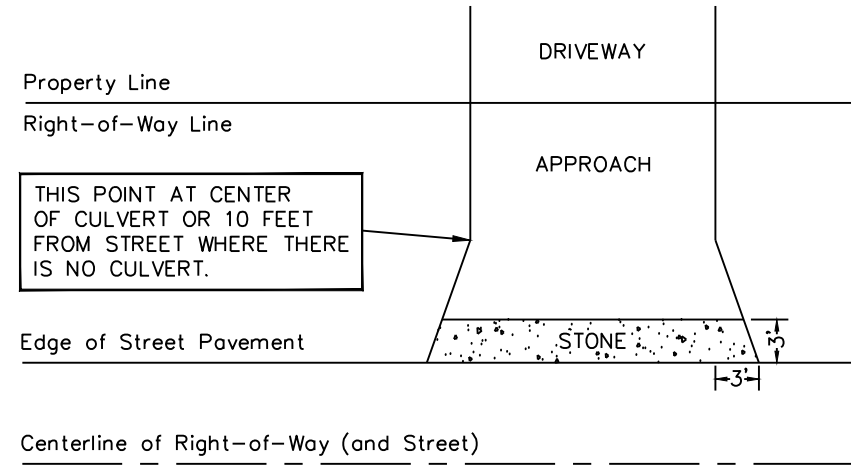
**RURAL-B DRIVEWAY APPROACH**

**RO-12**

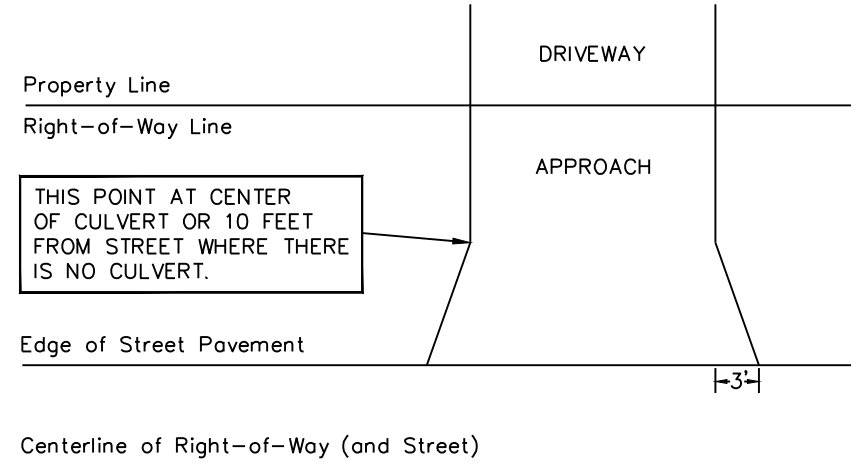
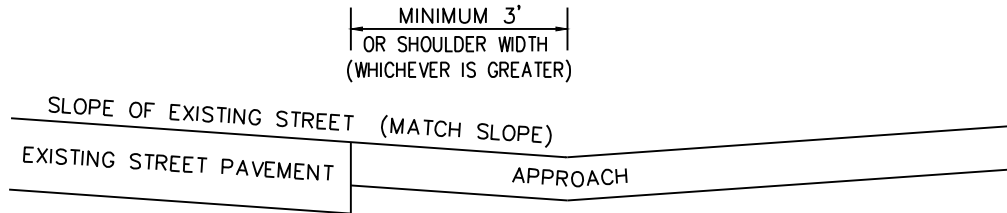
**STREET PAVEMENT NOT AT FINAL GRADE**  
*(BINDER COURSE IN PLACE, SURFACE COURSE NOT INSTALLED)*



ASPHALT WILL BE INSTALLED BY STREET PAVING CONTRACTOR AT TIME OF FINAL ASPHALT SURFACING.



**STREET PAVEMENT AT FINAL GRADE**



*City of Oak Creek*  
**Engineering Department**

Drawn By: A. WHITE

Scale: Not to Scale

Appv. By: M. SIMMONS

Date: 3/6/20 (Revised)

DRIVEWAY APPROACH TO CURBLESS PAVEMENT

RO-13